

2015



MINISTOX SPECIFICATIONS



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The formula is based on a 998cc Mini, as manufactured by British Leyland, **no other cars/bodyshells permitted to race alongside ORCi Ministox.**

No Cooper or Cooper S Engine/Suspension/Brake parts may be used.

VIOLATIONS

When referring to the engine, gearbox, differential, mechanical or construction, rules and regulations the principle will always be: unless permission is specifically granted to make modifications, or any variation, nothing may be done to alter or change in any way the standard Parts.

Unless these rules state you can do it, you CANNOT DO IT.

Violations or any refusal of a scrutineering check will result in an immediate suspension of all racing facilities.

All car and engine specifications will be taken from the Technical Service Data book as published by Glass's Guide Service Limited or the relevant Haynes Manual. Technical queries can be addressed to, The Secretary, ORCi Limited, Technical Committee, P.O.Box 9889, Birmingham, B43 6WA.

DRIVER REQUIREMENTS

The minimum age is 11 years old and all drivers will end on their 16th birthday.

However, a driver aged from 11-12 years may only race under the watchful eye of their promotion, with restricted bookings, until such time as the driver is confident enough to compete fully.

The driver's age must be painted on the passenger side of the car. A copy of the child's birth certificate must accompany their application for a licence.

New applicants are required to complete a multiple choice written test and a basic driving skills test before a licence will be issued. This is chargeable at £50 and must be arranged with your licencing Promotion at the time of application.

New drivers will be deemed to be 'ROOKIE' drivers for their first THREE meetings and must start at the rear of the grid and must paint the boot lid with a black or white cross. The Steward of the Meeting will be empowered to ask a driver to remove the cross once they feel that a reasonable level of confidence has been achieved. New drivers must only race at their home promotion until they are considered competent enough to compete fully.

No newly constructed car is permitted to race without being technically scrutineered, in advance of its first race meeting.

Drivers are reminded that scrutineering checks can be carried out at any time. If parts are suspected of being illegal you must leave them with the promotion if you do not agree. If you refuse it will automatically make the parts illegal.

It is the responsibility of the driver to prove to the ORCi that the part is legal by the way of written proof of where the part originated. This must be done within seven days, otherwise the part(s) in question will be deemed to be illegal and will result in immediate suspension from racing and referral for disciplinary action.

Only the registered driver or a responsible Adult is allowed to drive the car at any time within the confines of the raceway.

1. BODYSHELL PREPARATION

- Only Mini, Mini Clubman, Riley Elf or Wolsely Hornet Saloons are acceptable providing all run with 850cc/998cc running gear on the front and rear with a standard wheelbase of 2040mm + or – 25mm.
- Door tops maybe cut down as follows: passenger door maximum of 75mm and driver's door 40mm.
- All external lights, fittings and bright metal trim must be removed.
- The interior of the car must be completely gutted of all combustible materials, glass and trimmings.
- The bonnet/wings maybe ~~cut down and~~ welded together to achieve a one piece bonnet. **However, the bonnet must sit as originally intended, and not elevated.**
- The doors may be skinned, along with the rear pockets. The doors must be welded shut.
- Removal of the rear parcel shelf is permitted.
- Outer sills must remain as manufactured. A and B pillars must be retained. From the waistline down structural points on the A and B pillars maybe removed.
- Door skins (in between the upper and lower side rails) maybe replaced by sheet steel up to a maximum of 1.2mm thick.
- Dashboard bracing maybe removed.
- A steel boot floor must be in place on all cars. Though not necessarily original, this must tie the vehicle floor pan, rear arches and rear panel together and be welded in position. Further aluminium firewalls behind the driver are permitted and drainage holes in the area around the location of the fuel tanks are compulsory.
- Aerofoils are not permitted.
- The floor pan must be retained and made good, where the steelwork is joined to the external steelwork the body or floor pan must be made good.
- Inner wings may be removed.

2. WEIGHT

- Cars can be weighed at any time before, during or after a meeting, without driver. They must always achieve the minimum weight; the adding of fuel or any other liquids is not allowed to achieve this minimum weight, which is 720 kg. Racing weight must be achieved without the use of ballast.
- The right hand driver's side weight must be a maximum of 53.0%, at any time – this will be weighed without the driver in the car.
 - If you are checked and over the 53.0%, but under 53.5% on your first offence you will be loaded immediately with loss of any points gained (if applicable), and receive a final warning. You must have this written in your log book.
 - If you are found on a second occasion (during a calendar year) to be over 53.0% but under 53.5% you will receive a one month suspension, and loaded immediately with loss of any points gained (if applicable)..
 - Anyone over 53.5% on their first check will receive an automatic one month ban, and loaded immediately with loss of any points gained (if applicable).
 - If a driver is found over 53.5% on a second occasion (during a calendar year) the penalty will automatically be doubled, or possibly carry a longer suspension. Checks will be carried out at any time.

3. ENGINE, GEARBOX & DIFFERENTIAL

The regulations for the 998cc engine are as follows :

GENERAL

- You must not lighten, balance or stress relieve the pistons, con rods, crank shaft or flywheel. Only the standard factory balancing is permitted.
- All oil ways and galleries must remain unmodified (with exception of cam followers)
- **Aluminium rocker covers are permitted.**
- Engine mountings and bracing are free. With regard to engine mountings you may cut the sub-frame to accommodate a strengthened mounting, but no more than 75mm each side of the original locating bolt. Additionally the engine must remain in its original position and not be lowered in any way.
- A 50mm hole must be machined in the engine end cover in a convenient location for checking the flywheel.

ENGINE BLOCK

- The permitted engine/block code is 99H. The "A" Plus block is also permitted.
- The original bore size is 64.59mm with 1.5mm oversize allowed. The original stroke size is 76.2mm.
- Pistons must be standard 998cc replacement type (3 ring or 4 ring type permitted), with all rings in place. No machining of pistons allowed.
- Only standard replacement rings are allowed (No competition or total seal rings allowed).
- The block may be surface ground, but the piston must be level or down the bore.
- The block may be rebored or sleeved back to standard and rebored or line bored.
- No other alteration allowed.
- Any 998cc crankshaft is permitted, including the "A" Plus are allowed stroke is 76.2mm.

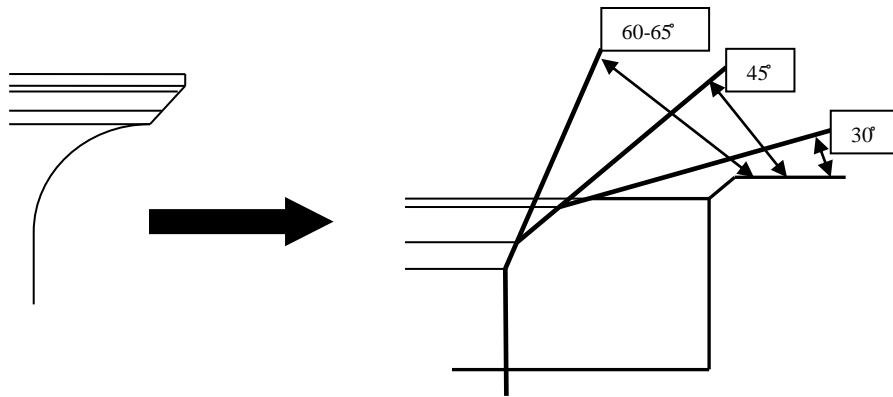
TIMING/IGNITION

- NO VERNIER PULLEYS ALLOWED.
- You may use off-set woodruff keys.
- Standard timing chain and tensioner must be used.
- Only the standard type distributor must be used.
- The advance may be altered and you may lock the base plate.
- No electronic ignition systems are allowed.
- Spark plugs are free.

CYLINDER HEAD INC. VALVES

- The head must be standard Mini 850cc or 998cc casting and may be surface ground to a minimum thickness measured between the top and bottom machined faces of 2,700 thou.
- Valves must be standard or replacement Mini 1000. The specified valve size, part numbers permitted are 2a628-2a629-12a1456-cam4810. DO NOT USE, 12g202/206/295 or 12G940 or 12A185 or AFG163.
- Valves must be of standard length and standard valve head size of 27.89 inlet, and 25.53 exhaust.
- Three angle valve seats are allowed to the following specifications but there must be no machining or fettling past the valve seat on the valve or the head.

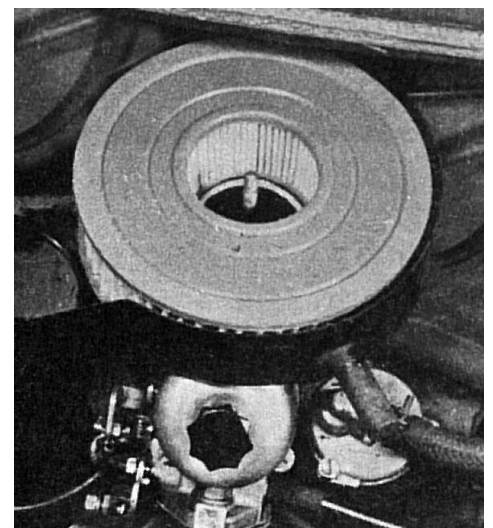
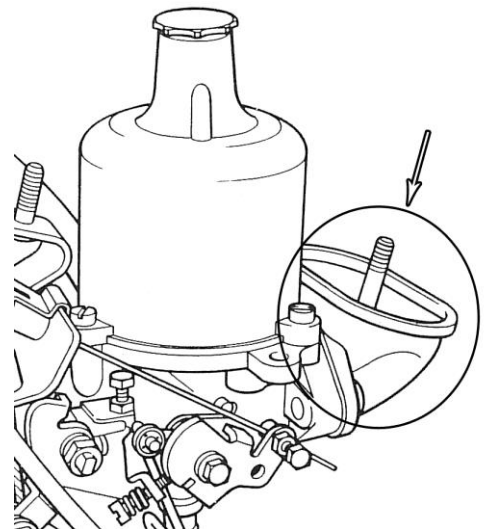
Three-angle Valve Seat Procedure



- Standard valve spring top and collets must be retained.
- Any single valve spring that fits without the need for machining may be used.
- Valve guides may be replaced or reconditioned using a thin wall bronze sleeve in existing guide, but must still occupy the original position.
- The combustion chambers and ports **must** not be modified in any way and must not have any material added or removed.
- Replacement valve inserts may be fitted, but must be level with the bottom of the combustion chamber. Inserts must not be fitted that enlarge the port orifice. Inserts must return the cylinder head to its original port diameter.
- Only the standard replacement gasket allowed.
- Cam followers, push rods and rockers must be standard Mini 998. **However, a maximum of two holes per cam follower are permitted for oil relief. These maybe drilled at any angle.**
- Any camshaft may be used that fits without additional modification to the engine or components.
- Rocker Gear: All factory types will be accepted with A ratio of 1.25 to 1.

CARBURETTOR/INLET MANIFOLD/AIR FILTER

- Only the standard Mini 1000 1½" carburettor, with the single outlet exhaust manifold is allowed.
- **These items must remain in their standard form. No machining is permitted, to include, but not exclusively, smoothing, cleaning or fettling, any of the flow areas i.e. carburettor, cast carburettor down pipe (as circled in diagram), carburettor insulating block, restrictor plate or inlet manifold.**
- The carburettor must be used with the restrictor plate provided by your ORCi Promoter (**stamped ORC15**). The plate must be fitted with the ORCi label at the top and fitted to the manifold with the holes provided. The restrictor plate must be fitted between the sandwich plate and the inlet manifold. Once fitted, you may modify the corners to allow throttle linkage clearance. **Drivers are to be reminded that any tampering with the supplied restrictor plate will automatically attract a 12 month ORCi racing ban.**
- Inlet manifold servo pipes must either be connected to the servo or blanked effectively.
- Jets and needles are free. No quick lift chamfers.
- **The original air cleaner/air filter and associated housing maybe removed. These items are shown in the lower photo on the right. Additionally, the cast carburettor down pipe as noted by the circle in the diagram maybe removed. Should the carburettor down pipe be retained it must remain completely in standard form, with no machining, i.e. the centre stud for mounting the original air cleaner housing must be retained (as shown in both diagrams/photos to the right).**
- **The only permitted air filter is a standard direct replacement part for the Mini 1000 pancake style filter. No high-flow cone type filters permitted.**
- **No ram/force air components are permitted anywhere within/inside/between carburettor/air cleaner housing/air filter/carburettor down pipe/carburettor insulation block, this includes, but not exclusively reducers, trumpets, cones, or inserts.**



RADIATOR

- The radiator must be mounted within the engine compartment and be of the standard type; **or an aftermarket aluminium radiator.**

GEARBOX/DIFFERENTIAL

- The only gearbox permitted for use is the standard Mini 1000 gearbox and all gears must be in place.
- The differential ratio must be 3.44:1. Gearbox type must be A+ 998cc.**
- Identification No DAM5626 (by oil filter). 2nd Gear ratio = 21 tooth layshaft and 26th tooth gear.**
- Final drive ration 3.44:1 = 18 tooth pinion and 62 tooth crown wheel.**
- Crankshaft gear = 29 teeth, Idler = 37 teeth and 1st motion gear = 29 teeth.**

NOTE: For 2015 at RDC Promoted tracks only - diff ratios are free

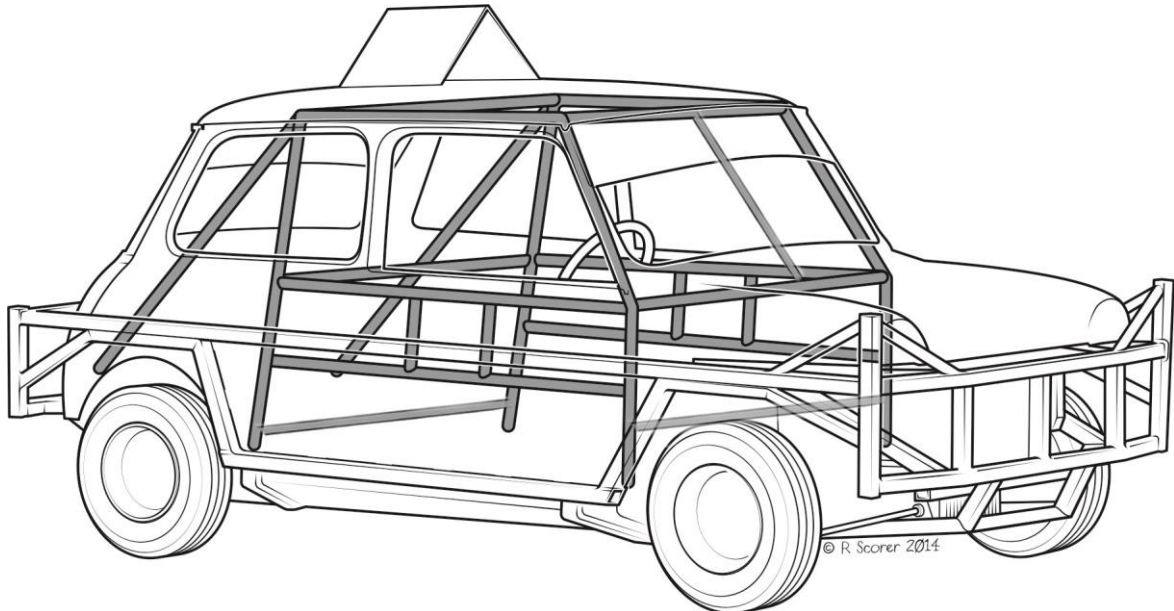
- All diffs must be unlocked, no limited slip or power lock types. A remote gear change is not permitted.

OIL PUMP/OIL FILTER ETC

- Oil Pumps: All three types of oil pump drive to suit the cam are permitted.
- Oil pickup pipe may be repositioned.
- No oil coolers or remote oil filter allowed.
- Compact oil filters are allowed.
- The engine must be fitted with a metal catch tank of 1 litre minimum capacity connected to the engine breather system. This must be fitted under the bonnet.

4. ROLL CAGE

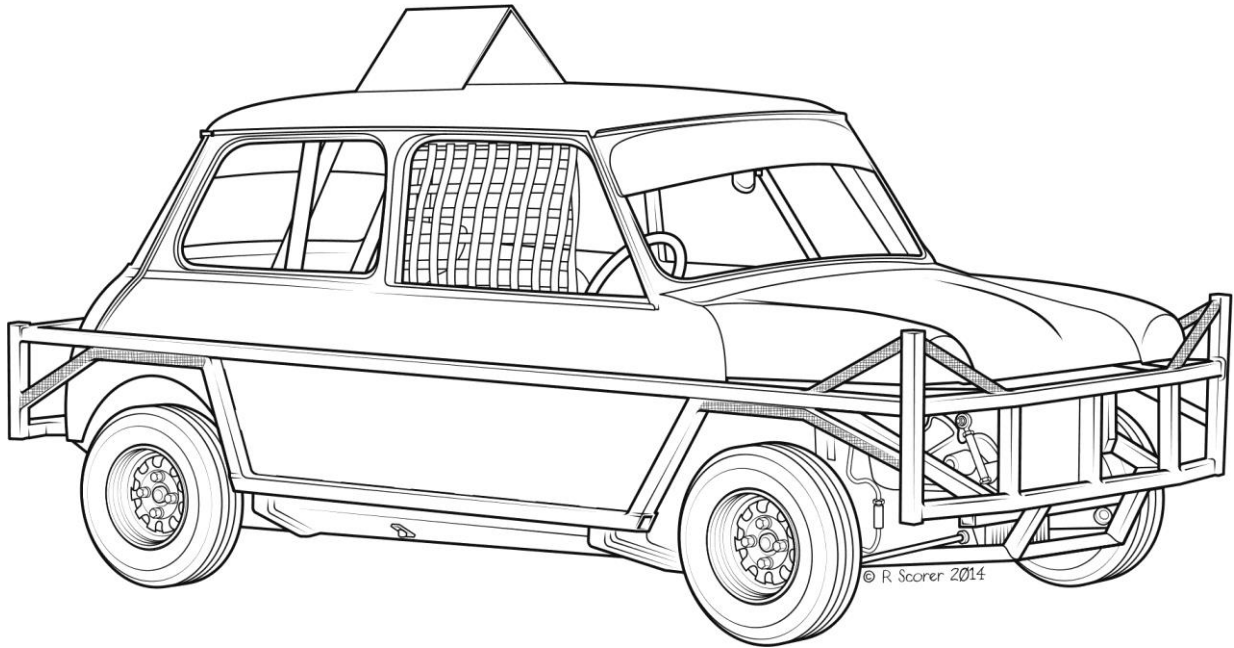
- Roll-cage/roof bars/chicken bars and all supports as noted below must be constructed of a minimum of 40mm x 40mm SHS or 38mm tube (3mm thick).**



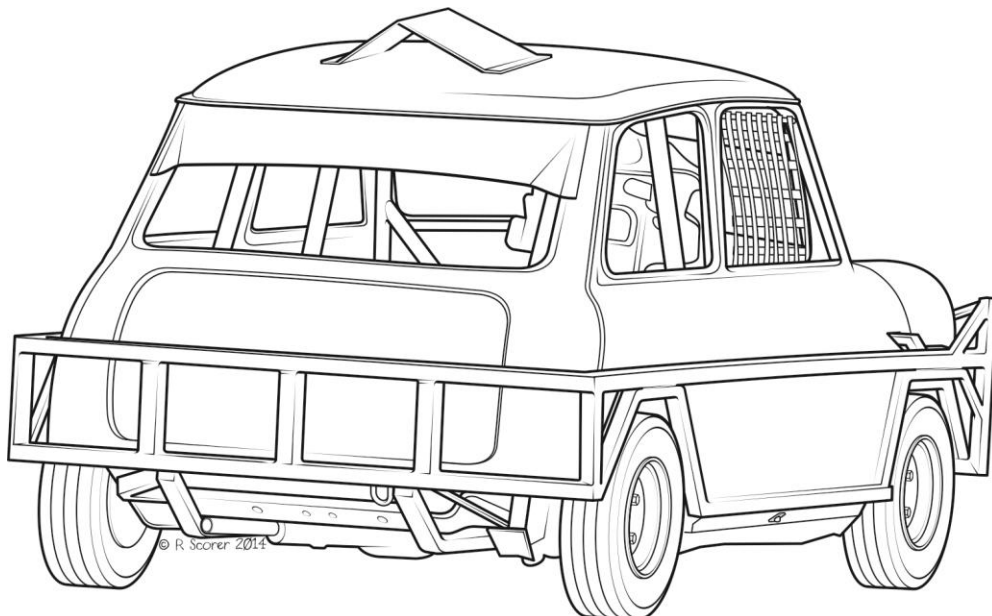
- The cage must consist of screen hoop and hoop behind driver, with two connecting bars in the roof between these two hoops, plus a centre bar to support the roof plate. If all four uprights are not welded to the main steelwork, they must have a steel plate on the base (minimum 100mm square, 3mm thick) bolted or welded to the floor. The cage must extend the full internal width of the car.
- The rollcage hoop located behind the driver must have two rearward support bars (one each side of the car) running from the **very top of the hoop (as shown in the diagram)** downwards to the internal steel work.
- Two chicken bars must be fitted horizontally to the roll-cage on both the driver's and passenger sides mounted between the A and B roll cage pillars (the sill bar is not considered to be a chicken bar). The chicken bars must have two vertical connecting bars between EACH one and must be symmetrical on both sides of the car. A 3mm plate must be welded to the outside of the chicken bars inside the car on the driver's side. The plate must extend across the full width of the door and must be a minimum of 250mm high.
- A horizontal bar must be fitted across the roll cage pillars at shoulder height and floor level behind the driver and at scuttle panel and floor level in front.
- A 3mm thick roof plate, on the driver's side of the car, must cover half the roll cage area. This must extend to the front roll cage hoop, rear roll cage hoop, driver's side roof bar and centre support bar.
- All roll cages must terminate within the vehicle.
- A metal upright min size 20mm x 20mm SHS must be welded or bolted in the windscreen aperture, at least one third of the way along from the driver's side. The use of steel mesh is permitted in the front windscreen, but must end half way across on the driver's side only.

5. EXTERNAL STEELWORK

- Bumpers/side rails/under rails must be constructed from steel 50mm x 30mm, 50mm x 25mm or 40mm x 40mm SHS or RHS, with a wall thickness of 3mm and must be constructed in accordance with the illustration below, to ensure even contact between cars. However for the steelwork shown below as shaded, the maximum specification steel remains the same, although 25mm SHS or equivalent tube as a minimum is permitted.



- All steel must remain hollow i.e. no ballast of any kind is permitted.
- The car must be symmetrical in construction side to side.
- The front bumper must consist of two horizontal bars, with either four or five vertical uprights. The height of the front bumper, measured between the top of the top bar and the bottom of the bottom bar must be a minimum of 230mm and a maximum of 360mm. The curve at front and rear corners of bumpers must not extend more than 150mm from the line of the side rail. Corner gussets up to 150mm of triangular 6mm thick plate may be fitted.
- The front bumper must also have a triangular extension on both sides, and this may be braced back to the top of the side rail, but no further back than the centre of the front wheel. The maximum size steel required for this is stated in the first paragraph of rule 5. However the minimum steel permitted for the triangular extension is 25mm SHS or equivalent tube.
- It is permitted to use a secondary piece of SHS or RHS steel to sit under the side rail, above the wheels only (shown as shaded in the above diagrams). The maximum size steel required for this is stated in the first paragraph of rule 5. However the minimum steel is 25mm SHS or equivalent tube.
- Diagonal bars from the side rail to the front bumper are permitted to the specification noted in Bullet 1 of rule 5.
- A gap between the internal and external side rails is not mandatory, and the amount of welding is free. However should the two rails have a gap between them, they must be joined at intervals in a ladder fashion.
- **NO EXTRA STEELWORK THAN THAT SHOWN IN THE DIAGRAMS IS PERMITTED ON THE EXTERIOR OF THE CAR.**



- Two through rails MUST be fitted underneath the car from front bumper to rear bumper, to hold sub frame in place. These rails must pass completely underneath the rear sub frame before returning up to the rear bumper.
- The steelwork may be used as a location for the front and rear sub-frame mounting brackets. Sub-frames may be strengthened by the use of steel plate or RHS. The front sub-frame must still have the large top mounting bolts removable and be in their original position. Sub-frames must retain their original sides (with the exception of Engine Mounts rule 3). The rear of the front sub frame maybe reduced to allow the removal of the steering rack.
- Attachments may be welded to the sub-frame from which to mount the bumper.
- Sump guards may be fitted but must be securely welded to the ironwork.
- The car must be no wider than 1475mm at its widest point.

6. INTERNAL STEELWORK

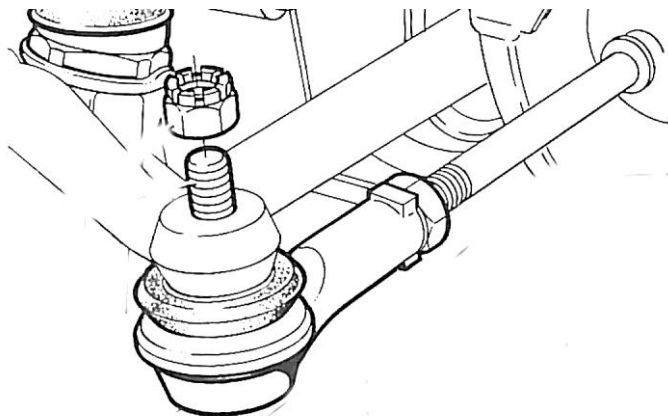
- All internal steelwork must be of equal proportions on both sides of the car in construction and thickness.
- The driver's floor pan may be reinforced with steel plate maximum thickness 3mm.
- Protective steelwork inside the car, rear of the engine compartment is free (with the exception of the Rollcage – rule 4). Any additional gussets must not exceed 80mm x 80mm x 6mm thickness.

7. WHEELS & TYRES

- Any standard manufacturers steel wheel 10" or 12" up to a maximum of 5" width may be used. No wheel spacers allowed.
- Tyres and wheels may be non-symmetrical on the front, i.e. 10 or 12" on either side. Only 10" wheels may be used on rear. Wheel studs may be modified to suit.
- Weller wheels etc. may be used with a maximum width of 5", a maximum inset of 3¼" and a maximum offset of 2¾". This measurement is taken from the back of the wheel rim to the back of the wheel stud flange face.
- Modified (stronger) wheel studs may be used on Weller Wheels, flanges may be modified to accommodate the different stud.
- Any tyre up to a maximum width of 6.2 or 165 may be used, with the exception of the following:
- No competition tyres. No re-grooved or Town & Country tyres.
- No remould tyres, no winter pattern and no ATV tyres.
- Yokohama AO32R, Yokohama A038R, and Yokohama A048R are not permitted irrespective of suffix. The Avon CR6ZZ, Dunlop Formula R and Kingpin Sportway Supagrip are not permitted.
- All identification marks must remain on tyres. No tyre flaps allowed. Buffing of tyres is not allowed. Tyre softener is not allowed.
- Tyres must not be re-grooved.

8. SUSPENSION - FRONT

- Suspension components must remain standard. Suspension cones and ball sockets must be fitted unmodified and must be identical. (Adjustment of ride height may be achieved by adding washers).
- The only shock absorber permitted for use is KYB, part number 442001. Manufacturers' markings must not be removed.
- You may lengthen the near side track control arms by up to a maximum of 20mm in length to give a maximum of 4° negative camber.
- The maximum permitted camber **on the OSF wheel is 0.5°**. You may shorten or lengthen the near side front tie rod. The modified components are not permitted to be adjustable on the vehicle.
- All suspension parts must be mounted in the original position and must not be attached to any additional ironwork. All cars must have suspension movement up and down. No lowering allowed.
- Push fit drive shafts may be lengthened up to 25mm to prevent driveshafts popping out on impact.
- Ride height will be measured on the driver's side of the car, from the ground to the underside of the roof gutter at the 'B' pillar; it must not be less than 1170mm at any time.
- The steering rack may be lengthened on the near-side only, by up to a maximum of 20mm. Alternatively the track rod ends may be changed to accommodate a longer track control arm, however these must be mounted as standard i.e. **the rod end must mount from the underside facing upward (as per picture)**
- The repositioning of the near side front top shock absorber mounting is not allowed.
- Bump stops must not be removed, and must be of the non-competition type.
- The standard Mini steering column may be modified or repositioned to suit the driver, quick release steering wheels are permitted.



9. SUSPENSION – REAR

- The only shock absorber permitted for use is KYB part number 442002. Manufacturers' markings must not be removed.
- The rear suspension must always be set within the manufacturers tolerances.
- ***The rear wheels are permitted a maximum of 0.5° of camber.***
- All suspension parts must be mounted in the original position; however sub-frame mounting points may be attached to the steelwork. (Adjustment of ride height may be achieved by adding washers).

10. FUEL SYSTEM & FUEL

- A full firewall must be in place between the driver and fuel tank and engine bay, with all holes blanked off. Where additional internal panelling is used, these must be detachable to allow full visible access.
- ***Only tanks with a maximum capacity of 2 gallons are permitted, these must be fitted inside the boot area and completely on top of the steelwork, where they are separated from the driver and securely mounted.***
- All tank filler caps must be of metal and screw fixing.
- Petrol pipes must be of metal, metal covered or specific rubber braided fuel hose and have a shut off tap within easy reach of the driver. Reminder that rubber connecting hoses on fuel lines are not permitted
- All tanks must be fitted with a breather system, which prevents spillage if a car is inverted.
- All petrol pick up pipes must draw through a stand pipe from the top of the fuel tank.
- A non-return valve is compulsory in the breather pipe.
- Fuel lines must be clipped securely, and routed away from electrics i.e. if a fuel line runs along the inside edge of the chassis rail, you may run the electrics along with outside edge of the rail, as a minimum.
- Electric fuel pumps can be used in place of the standard manual pump but they must be situated in the boot area behind the fire wall. 4 x 50mm holes must be drilled at the lowest point under the fuel tank to allow spilled fuel to drain.
- The use of in-line fuel pressure regulators is allowed.
- For the full fuel specification please see separate sheet.

11. BATTERIES & ELECTRICAL

- Batteries must be securely clamped in place behind the driver with a leak proof material to prevent spillage of corrosive liquid.
- An electrical cut/off switch must be fitted to the R/N/S corner of the car.
- If a car is fitted with an electric fuel pump, a switch must also be within easy reach of the driver.
- Self starter motors must be fitted and in working order at all times.
- Gel type batteries are permitted.

12. BRAKES

- Brakes must be effective on all four wheels. A handbrake must be fitted and in working order.
- No Cooper or Cooper S parts may be used.
- Two brake lights must be fitted and working to be able to race on non contact tracks. These lights must be disconnected when racing on contact tracks. (See Rule 12 for Brake Light Specifications).
- Mini 1000 front disc brakes (not Cooper or Cooper S) are permitted ***on the front of the car***, and you may skim the callipers to allow 10" wheels. ***If using disc brakes, they must be fitted on both front wheels.***
- The use of standard Mini 1000 spacered drums, on the rear only are allowed, provided they are fitted to both sides.
- Brake servos may be fitted as standard or remote, but it is not permitted to fit a servo which requires changes to the pedal box etc. If the brake servo is fitted it must be in full working order. If the brake servo is not fitted, then the servo pipe must be blanked off by way of WELDING only.

13. TRANSPONDERS

- An AMB Tranx260 DP transponder for lap scoring is mandatory and must be working at all times.
- It must be fitted 1.8 metres back from the front most position of the car. A hole of at least 150mm square is required in the floor, with the transponder fitted vertically, just above the hole.
- In the event of a dispute with the transponder result, the Steward of the meeting will make the final decision, however, if the driver is found to have fitted the transponder further forward than the required 1.8 metres, then the driver will be excluded from the meeting.

14. SEATS

- Fibre glass seats and other specialist racing seats are highly recommended, and must be securely fitted.
- The back of the seat must be adequately supported and provide a strong head restraint otherwise this restraint must be an integral part of the roll cage.
- Driver's seat may be moved up to 75mm from original position towards the middle of the car.
- A 75mm x 6mm flat steel plate shaped to the seat at muster height and welded to the roll cage must be fitted as support for the seat.

15. NUMBERS

- Numbers must be displayed on both sides of the car and on a roof fin plate visible entirely above the gutter line.
- All numbers must be of professional appearance, painted black on a minimum white background of 50mm.
- Regulation side numbers must be 300mm high in 50mm strokes.
- Regulation fin numbers must be 230mm high in 40mm strokes.
- Aerofoils are not permitted.

16. SIGN WRITING

- The driver's name must appear plainly on both sides of the car. Only other writing confined to sponsors or mechanics names. Childish drawings are not permitted.
- The drivers age must be painted on the side of the car above the number.

17. ROOF COLOURS/GRADING

- When notified of their grading, drivers must paint the entire roof of their car in their appropriate colour, White, Yellow, Blue or Red, down to the gutter line. No superstar grade.
- The current Points Champion must paint their car with a silver roof.
- The current points leader must paint their roof with a silver stripe.
- The following championship roof grades only are permitted: British – black and white chequered, National – maximum 300mm gold stripe, ORCi – orange and white chequered.
- The silver roof, silver striped roof cars and champions noted above must start at the back of the grid.
- Only the correct roof grade and racing numbers may be painted on the roof. NO SIGNWRITING.
- Driver's name must be painted on a sun visor bolted or welded to the top of the windscreen aperture.
- ***Where drivers are licenced with more than one Promoter, the highest grade currently held will apply to ALL Promotions that the drivers is registered with. This is the drivers responsibility and failure to start in the correct grade will result in exclusion from the race and may attract further penalty.***

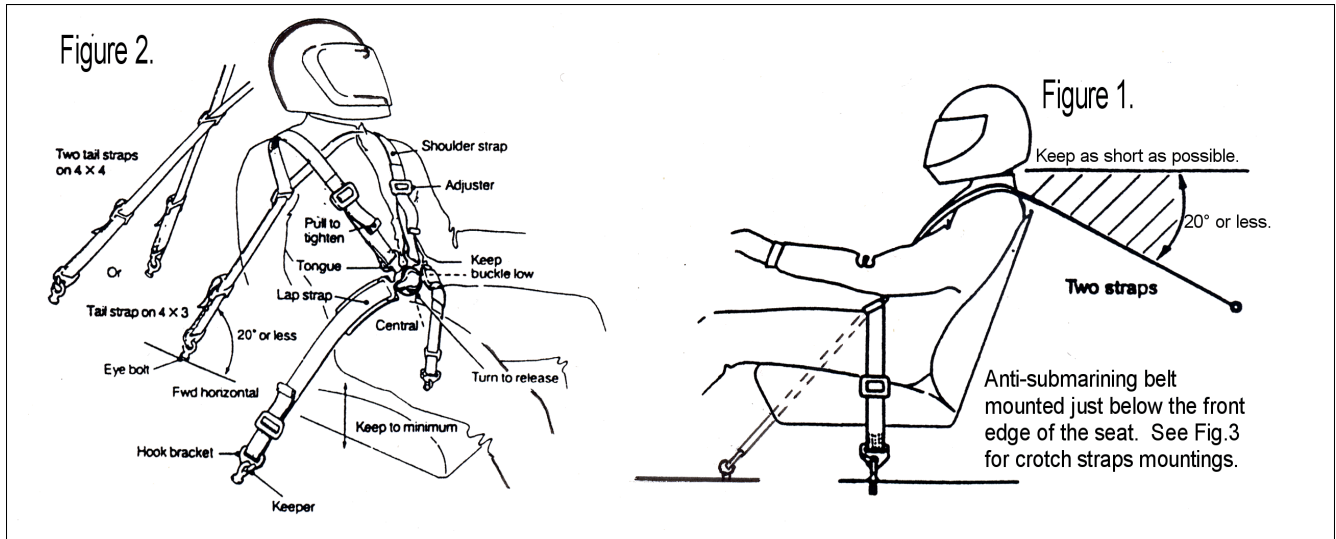
18. EXHAUSTS

- Exhaust system must be a complete standard Mini 1000 system, which terminates to the passenger side as original. This can be either a one box or two box system.
- This must be a standard bore exhaust pipe max OD of pipe is 1¼" (32mm). The rear box must be located within the confines of the rear sub frame as standard and must also have a minimum of a 50mm length tail pipe remaining on the rear silencer box. The lagging of exhaust systems is not permitted.

19. SAFETY EQUIPMENT

- Helmets must be of a minimum standard as directed by British Oval Racing Safety Executive (B.O.R.S.E). **NO POLYCARBONATE helmets are allowed.**
- The following standard of helmets are permitted: FIA8860-2004, Snell SA2005, Snell SA2010, SFI Foundation 31.1A, SFI Foundation 31.2A. The E2205 European standard helmet may be used in Fibreglass, Carbon or Tri-Composite form only.
- It is important that the helmet fits the driver correctly.
- Shatterproof goggles/visors must be worn although tinted visors are not advisable. The helmet must display the current ORCi sticker.
- Neck braces are **MANDATORY**.
- Fire retardant gloves and balaclavas are **MANDATORY** and must be marked appropriately.
- Drivers must wear bright coloured racing overall type clothing of flame retardant Proban or a high specification material and this must be maintained in a clean and tidy condition in view of the public. N.B. If wet weather clothing is used this must be worn IN ADDITION TO and NOT INSTEAD OF the regulation flame retardant overall type of clothing described above.
- A quick release cloth window net must be fitted to the driver's door window aperture. The netting must have holes not larger than 75mm wide. It must come down level with the steering wheel, and must be flexible and easily removable separate to the movement of the door.
- A 1Kg Dry Powder Gauge Fire Extinguisher is highly recommended and if fitted, this must be in a tube with a spring top and must be within easy reach of the driver. Old type BCF (green) type extinguishers are not allowed. All tow vehicles, must carry a minimum of a 2kg fire extinguisher dry powder or gas, which must be within easy reach of the driver and mechanics at all times, especially when refueling.
- A full five point buckle release harness (including NASCAR type) with sub-strap must be fitted and bolted to the floor and/or the roll cage. Shoulder belts with a sternum protection latch are highly recommended. The sub-strap must be used at all times and all belts must connect to the quick release buckle. In the case of NASCAR lever latch buckles it is advisable to fit a secondary means of detent to prevent overall sleeves accidentally unhooking buckles during racing. A small section of Tubegrip elasticated bandage slid over the hooked buckle serves this purpose. Special attention must be paid to the condition of seat belts and fixings once fitted. An extra bar is to be fitted to roll cage behind driver's seat approx 100mm below shoulder height of driver. Your seat belts may be fixed to this bar. The bar is to be of roll cage material specification.

Please study the diagrams below to ensure your safety harness is fitted correctly.



20. GENERAL RULES OF RACING

- Each driver is only permitted one car per meeting, and each car is only permitted one driver per meeting. **A responsible adult must be present at each race meeting with a Mini Stox driver.**
- Drivers may only use **limited** force to push a car off its line, or spin a car towards the infield. The following are not allowed: deliberate or forceful fencing, side swiping or spinning a car towards the racing barrier.
- New drivers must paint the boot lid with a black or white cross. This will mean that other drivers may not make contact with them, and that they will not be allowed to make contact with other competitors.
- Please note that all of the above rules are subject to change in respect of Health & Safety requirements.

REVISED RULES FOR 2015 IN RED/BOLD/ITALIC PRINT

ITEMS MARKED ~~STRIKETHROUGH~~ ARE NO LONGER ALLOWED

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